

JAGUAR ASSOCIATION OF NEW ENGLAND

COVENTRY CAT

VOLUME 5, NUMBER 2

FEBRUARY 2003



MK IV DHC, XK140, MK X and MK IV 1.5L

INSIDE THIS ISSUE:

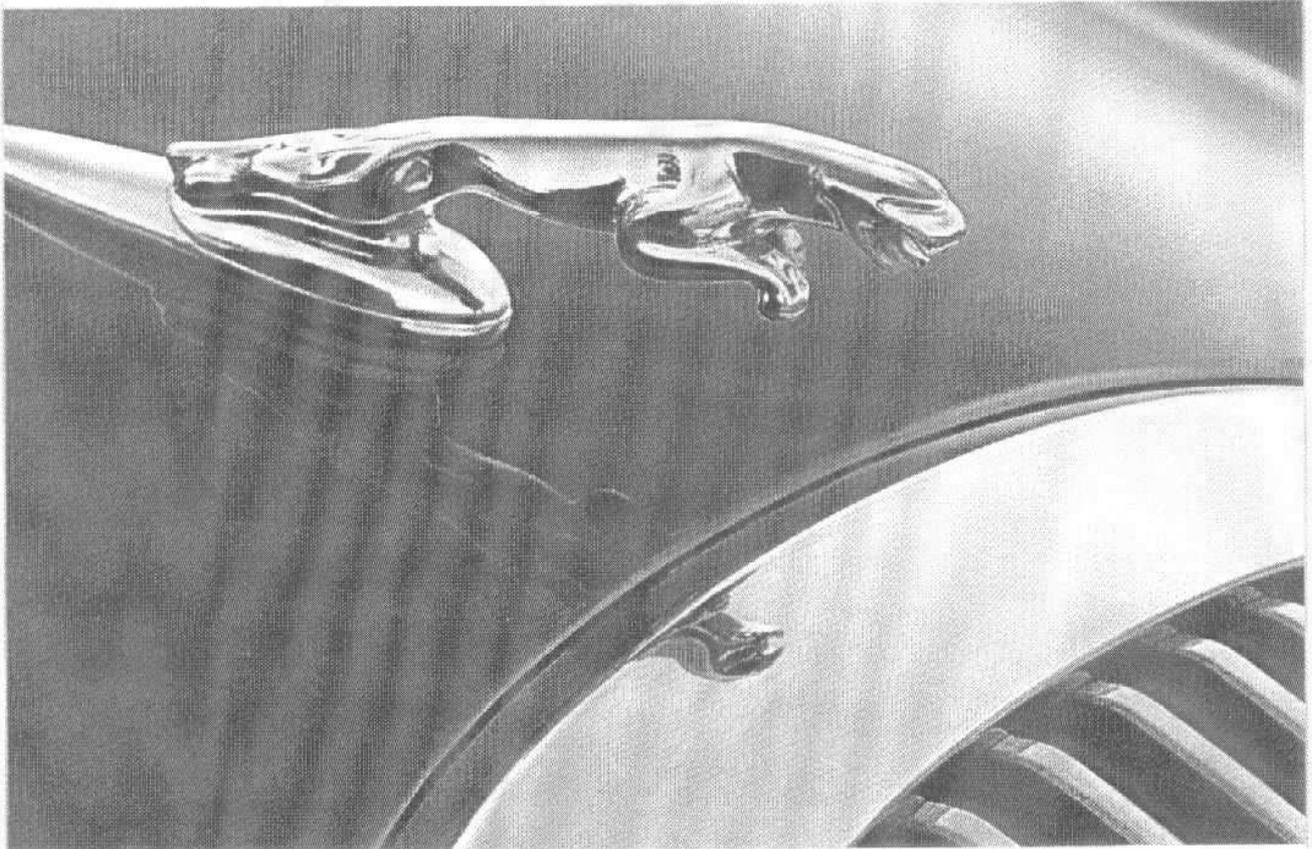
THE PREZ SEZ	1
NIKI LAUDA AND FI	3
CARL'S RESTORATION	4
NORMAN DEWIS	6



INSIDE THIS ISSUE:

EVENT PICTURES	7
EVENT SCHEDULE	8
SALEM WITCH HUNT	9
CLASSIFIEDS	10
LETTERS	11


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Jaguar Association of New England 2003 Calendar of Events

Events for the year which JANE will host, co-host or participate in as a club. All members are always welcome and if you have an idea for additional monthly events please contact Wes Keyes 207-363-5338 or Dave Higginbottom 978-683-6278

February

Monthly Meeting, Skip's, Chelmsford, MA.....February 26th

March

Restorations, Dave Roth/Harry Parkinson ,

Hollis, NH.....March 1st

Monthly Meeting, Skip's, Chelmsford, MA.....March 26th

April

Judge's Session, Chicopee, MA.....April 5th

Monthly Meeting, Skip's, Chelmsford, MA.....April 23rd

Have an idea for an event ?

Give Wes a call

JANE OFFICERS

President:	Carl Hanson	(781 275 2707)
VP/ Events:	Wes Keyes	(207-363-5338)
Slaloms:	John Loring	(603 394 7525)
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President's Notes

The new year of JANE activities has begun. Our membership continues to grow and our events continue to please. What more could we want? To tell the truth, we could use some warmer weather. This winter has been brutal – making our garages a mite chilly for constructive work on our dormant vehicles. I recently visited a fellow Jaguar enthusiast in Florida during a business trip and turned green with envy when he met me at my hotel in his XK120 FHC. I turned two shades of green, in fact – one being green with envy that he could drive his car in January and two being green with envy that he has a running FHC! The first we in New England have no control over, other than moving to a warmer climate; the second is my personal problem which I hope will be solved in due time. Don't ask me *when*, however. It is becoming a touchy subject!

By the time you read this, we will know the outcome of the first Board meeting under the new By-laws. The reduced size of the Board is of concern to some. To others, it is the right approach. We will work with it this year as an experiment. I am confident that our meetings have sufficient interest for social interaction that we will continue to attract twenty or more people to Skip's Restaurant on the fourth Wednesday of every month. Note that I said "twenty" people, which is the usual turnout to a JANE monthly meeting. In fact, "twenty" is about average for attendance at any given event. What is magic about "twenty" people? That is not many, considering the fact that JANE has over 300 members. I would love to see that "twenty" number go to "thirty" and "forty" this year!

The month of February has no event planned as of this writing, other than the usual monthly meeting at Skip's Restaurant in Chelmsford. If you want to help us by hosting something this month, please call Wes Keyes. And of course, everyone is invited to the monthly meetings for fun and fellowship and to hear how the club runs its business. Close on the heels of this short month, on March 1st, we will be visiting two exciting restoration projects in Hollis, NH. Harry Parkinson's Mk IV project is progressing nicely, as is David Roth's early saloon. (Saloon car, that is. He may be making progress on a saloon also, another subject dear to the heart of a true Jaguar aficionado, but he has not admitted such.) This may be the year for the coming out parties of long-standing restoration projects. These guys are early on the schedule for this year. Come and see their projects! Both promise a *warm* reception!

That is enough for this month. Keep warm!

Carl

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FORMULA 1 UPDATE**NIKI LAUDA SACKED!!!***By – Linda Young, Editor,**The Cat Tale, Jaguar Club of Tulsa*

Those who have read my rants about Niki Lauda and the Jaguar formula 1 team know that I have never been a fan of Lauda, to put it mildly. Yes, he was a great driver but time and again he proved that he has no managerial skills. Richard Parry-Jones head of Jaguars F1 team tried to put the best face on the situation, saying the Lauda would be "offered a position as special advisor to me."

Lauda, in typical style, was insolent and arrogant as he told Austrian radio: "Honestly, the decision did surprise me -- there was no criticism of the way I work, or whatever...But what you've got to know is that in England things work differently. Britons do have their unique way of solving problems. They saw away at the legs of a chair -- nobody talks about anything because they of course all stick together -- until they get what they want. I was always aware that that's how it works here -- and now it's happened. I am neither an engineer nor an Englishman and that's why it's easy for me to accept (being replaced).

"I've now got to consider whether to accept their offer of continuing to work for Ford."

Gosh! That is really big of you, Niki!

Now Tony Purnell who started out as an aerodynamicist with Lola and is now part of the Jaguar team hierarchy, will temporarily fill the role of Team Principal until he finds a suitable replacement for Lauda. Purnell emphasized the breadth as well as the depth in the changes being made.

"For Jaguar Racing, we need to build 'the Jaguar way' of producing Formula One cars. This means not only that we have the right technical people in place but that we have the managers and support structures that make it a happy place for the engineers to work in."

Meanwhile other changes have taken place. Eddie Irvine was not offered a new contract and according to Eddie, Niki was pretty darn rude about that too! Pedro de la Rosa had his remaining contract bought out. While it was time for Eddie to go, I don't think Pedro got a fair chance at Jaguar. He obviously was not comfortable with the self serving egotism of Lauda or the mouthy showboat, Irvine. Perhaps Pedro will get a drive for another team and time to prove his worth.

Meanwhile, back at Milton Keynes, the new boys Mark Webber and Antonio Pizzonia are testing the new R-3 at Barcelona and doing quite well. Perhaps the change in personnel improved the general moral of the team. Let's hope that Jaguar is getting a fresh start after shooting themselves in the foot by releasing Bobby Rahal. Loyal Jaguar fans want to see the leaping cat at the front of the pack again.

We want to see a return to the glory days of C-type and D-type domination in F1.

Maybe now that Niki is gone someone can do something about Bernie Ecclestone before F1 is completely ruined.

NEWS FLASH* *Barcelona, Spain – Nov 29, 2003*

Pizzonia rolls S-Type at Barcelona

Antonio Pizzonia and three passengers walked away unhurt after the young Brazilian rolled a Jaguar S-Type R super saloon at the Barcelona circuit on Thursday. Pizzonia, who began his official test duties as a works driver for the Leaping Cat this week, was participating in a promotional display after he had completed his time in the F1 cockpit. He went into the Circuit de Catalunya's first right-hander too fast and barrel-rolled the \$69,000 400-horsepower machine at well over 200 km/h. But thanks to the top-level safety systems employed in modern cars, neither Pizzonia nor the three passengers were hurt even though they were given precautionary checks in on-site medical facility. Despite doing its bit to ensure its occupants were safe, the S-Type was written off in the incident having had its roof partly collapse in the impact. The incident will not affect Pizzonia's scheduled testing at the circuit today.

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RESTORATIONS

by Carl Hanson

Gearbox rebuild

Last month I described the joy experienced when a long-dormant XK120 engine comes back to life! Next step is the gearbox. I figured that while everything is out of the car I might as well have Brian Donovan put the gearbox into top shape. After all, nobody has driven this car for a whole lot of years. The condition of the drivetrain is totally unknown.

Moss gearboxes used in XK's are known for being strong, although they are somewhat slow to shift since they have big and heavy gears. The non-synchronization of shifting down to 1st gear takes some getting used to – the art of double clutching comes in handy during spirited driving! But there is one weakness that has generated lots of stories: older transmissions tend to get stuck in first gear. Bruce Cunningham had this happen to him while using first gear as a brake coming down a mountain road in North Carolina on the way to the Jaguar Challenge Championship in Franklin, TN. Rob Reilly from Chicago got locked into first gear the very first time he drove his 1951 XK120 FHC. Other stories abound.

Donovan says he has a solution. When it was new, the first gear had a pin to prevent the synchro collar from moving too far. But this pin was not very hard and wore down with use. One solution I have seen is to place a spot of weldment onto the pin location. Welds are hard enough to prevent premature wear, all right, but great care has to be taken to avoid dropping weld spatter onto those smooth gear surfaces. Brian's solution is a collar that he places on the shift rod so the slider cannot run back too far. Sounds like a good solution to me.

Brian stripped, inspected and fully re-built the gearbox with all new bearings, seals, gaskets, and lay shaft. The layshaft is a commonly replaced part that shows lots of wear. And it is available in new condition. Which is more than one can say for most of the gears. One cannot obtain new gears for these early gearboxes. Brian says the last set of new gears he got for an XK was five years ago, and cost over \$1,500 for a set. He tries to re-use all the existing gears. If an inspection prior to a rebuild shows that gears are excessively worn, then he scrounges around for the best available used set. So hang on to those old gearboxes. They may be needed for a transplant at some future date!

The last piece is to assemble the gearbox and attach it to the bell housing and to the engine. The completed unit is now ready to be loaded into my truck and hauled home to the awaiting chassis!

Figure 1. Brian Donovan points out a feature of the bell housing.



Figure 2. Shiny and Bright! The complex pieces of the gearbox.



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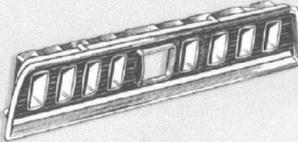
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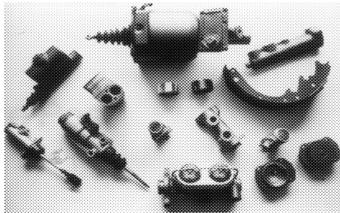
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More on Norman Dewis and the Jabbeke Record Run

By Mark Stephenson

From: "Mark Stephenson" <marks@jaguarot.com>

To: <xk@jag-lovers.org>

Subject: [xk] Norman Dewis

Date: Monday, December 30, 2002 4:01 PM

More on the Jabbeke record run and Norman Dewis..... The Belgian authorities at least had the sense to permit the test runs only in the early morning hours (and perhaps only on weekends) which is why the other side of the road is almost deserted.

Imagine being that guy on the motorcycle putting along at some moderate speed and seeing a streak fly by on the other side.

It's interesting to note that Norman Dewis had an uncanny ability (probably still does) to be able to sort out a car by the seat of his pants. He'd push a car to it's limits and then be able to tell the engineers what needed to be done to put it in proper nick.

I believe he was the first person to push the brake pedal on disc brakes. That was a year or two before the Jabbeke run. Perhaps some of the home country listers can fill in the details, but apparently MIRA had a test track with a long straightaway that ended in a tight left-hander. I'm not sure they considered off-road components for the C-type, but Norman reported that early on, more often than not, he'd come over a slight rise, step on the brakes for the corner, and ...nothing -- pedal to the floor. Fortunately they had a large run-off area which he visited regularly.

He raced the C-types for a few years. I don't know if he ever competed in the Ds. He set them up and sorted them out, but he was too valuable to the factory as a test driver to risk losing on the track. He came close a few times anyway. He was at the wheel of the XJ-13 in 1970(?) when it cart wheeled and rolled horrifically after a wheel or suspension failure at the official press introduction of the V12. Norman felt it let go and when there was no saving it, he just ducked down as far as he could and went along for the ride.

He was the chief test engineer for everything up through the Series III XJ6, I believe.

He hasn't lost his racing instincts either. A couple or three years ago at the Monterey Historics, Norman was out on Laguna Seca in a C-type. A cameraman and driver in a late model Merc were following behind trying to film his historic lap around the track. Norman couldn't stand the dottering pace, so he stepped on it. The Merc tried to keep up, but lost it and spun out.

The guy who probably knows the most about Norman, besides Norman, is Terry Larson. Although separated by a generation the two have traveled extensively together, a couple

goodwill ambassadors for Jaguar. He's one of, if not the last, of the famous post-war names at Jaguar.

By Mark Stephenson,

Jaguar Club of Central Arizona

P.S. He and an all-star cast of Jaguar management were in the factory when the fire broke out in '57. He related that experience to us at our concours a few years ago, too, and I've passed that on the list a time or two. It should be in the archives.

UPCOMING EVENTS

RESTORATIONS

On March 1st, a tour of southern NH garages is planned for "show and tell" of Dave Roth's S type and Harry Parkinson's MK IV. Both cars should be on the road for the upcoming season so they're nearing completion. It's a chance to do some "bench racing" and get some questions answered for your projects.

After the garage tour, there will be a lunch at the Roths' before going home to our own projects with renewed enthusiasm. There will be a nominal fee for the lunch, Contact Dave Roth at 603-465-3787 or Harry Parkinson at 603-882-9367 (Harryparkinson@attbi.com) for more details.

Judging Jaguars

A tech session on judging cars will be held on April 5th at Chicopee, MA (Hoppe Tool) on judging. JCNA rules will be reviewed and a practice run of the judging including scoring will be conducted.

Lunch will be included and participants will receive the latest JCNA rules. The indoor event will be held rain or shine.

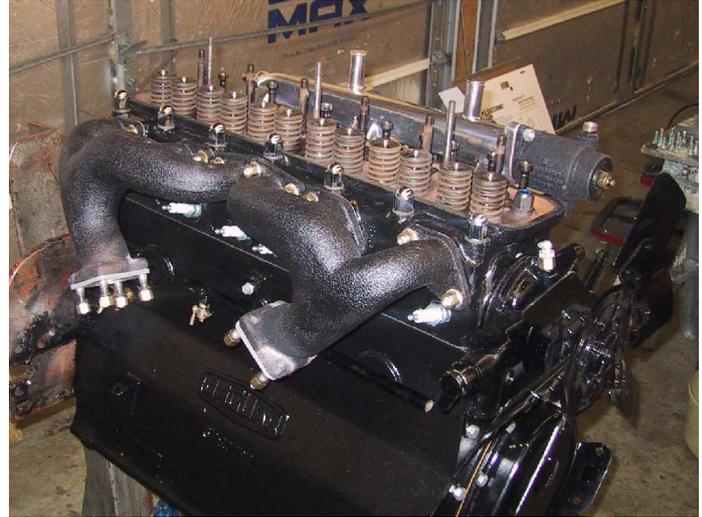
It is hoped that our fellow JCSNE members will join us. All participants will be classified as official JCNA judges,

Contact Harry Parkinson at 603-882-9367 or Harryparkinson@attbi.com for details

Member's Pictures



Wes Keyes changing a XJ6 water pump at Alan Quinn's garage



Early Jaguar pushrod 3.5L engine being restored at Reggie Ray's in Pennsylvania



Bill and Pat Stark with their XK140 FHC



Presenting a check to David's House



A Bugatti Type 51 GP on display at Williamsons



Roger Barber's XK120 FHC at Castle Hill

Activities Calender

Date	Event	Location
February 11, 2003	JCSNE meeting, Olympia Diner	Berlin, CT
February 24, 2003	JANE BOD meeting	Skip's, Chelmsford, MA
March 1, 2003	Restoration show and tell, Dave Roth's 340, Harry Parkinson's MK IV	Hollis, NH
March 11, 2003	JCSNE meeting, Olympia Diner	Berlin, CT
March 15, 2003	Mini-concours Memorabilia, JCSNE	
March 20-23, 2003	JCNA AGM	Charlotte, NC
March 26, 2003	JANE BOD meeting	Skip's, Chelmsford, MA
April 1, 2003,	April Fool drag @ NE Drag Way, try your car out on strip	minimal expense
April 5, 2003	Judging Cars/JCNA judging rules	Hoppe Tool, Chicopee, MA
April 8, 2003	JCSNE meeting	TBD
April 30, 2003	JANE BOD meeting	Skip's, Chelmsford, MA
May 1-5	JCNA Challenge Championship	Phoenix, AZ
May 2, 2003	Bonham Classic Car Auction	MOT, Brookline, MA
May 6, 2003	Lecture and Cookout on the lawn	MOT, Brookline, MA
May 13, 2003	JCSNE meeting	TBD
May 18, 2003	Slalom @ Site	TBD
May 28, 2003	JANE BOD meeting	Skip's, Chelmsford, MA
May 30-31, 2003	Jaguar Club Event, Track time	LRP , Lime Rock, CT
June 7, 2003	Amalfi Racing Tour	
June 7-8, 2003	JCSNE/JCNA Concours	Berlin, CT
June 14-15, 2003	Overnight trip to the White Mountains	
June 29, 2003	British Car Day	MOT, Brookline, MA
July 12, 2003	Carburetor Tech Session	Sunapee, NH
July 19, 2003	Automotive Machine Shop Tech Session, Berkshire tour	Greenfield, MA
August 9-11 2003	JANE/JCNA Concours	Sturbridge, MA
September 14, 2003	Slalom @ Site	TBD
September 18-21, 2003	British Invasion, JCNA Concours	Stow, VT
September 29, 2003	Castle Hill Concours	Ipswich, MA
October 30, 2003	Maine Museum Tour, Coastal Tour	
4 th Wednesday, each month	JANE BOD meeting	Skip's, Chelmsford, MA
2 nd Tuesday, each month	JCSNE meeting	Olympia Diner, Berlin CT

Wes Keyes

Activities Chairman

westonkeyes@hotmail.com

207-363-5338

Combined JANE/JCSNE/JCNA events

SALEM WITCH TOUR

Text and photos by Chuck Centore

Salem Witch Tour Sept 2002

On a windy, early fall afternoon, under threatening New England skies, a brave group of JANE members brought their Cats out for a tour of historic Salem Massachusetts. This trip included a visit to the Salem Witch Museum and a coastal tour through some of Boston's nicest North Shore seaside towns. Biff Michaud, Executive Director of the Salem Witch Museum & JANE member was our Host & Guide.

Our day began with a visit to the Salem Witch Museum. Biff took special care to see to it that our group was well cared for as he recounted many interesting facts about those early times in Salem, the Witch Trials and how they affected our present day jurisprudence system. After the Museum Tour, members were free to shop and enjoy the area around the museum.

Then without warning, thirteen Cats (a little poetic license, there were only nine cat' s, 1 Cadillac, 1 Volvo, 1 Toyota, & a GMC SUV), toured Route 127, New England's Coastal Highway. We made several stops along the way to enjoy the scenery and listen to Biff recount many fascinating facts about New England's history. At one point we actually raised our umbrellas as a salute to the threatening skies. (It was actually raining for a brief moment or two.)

The tour took us through Manchester, Marblehead and Magnolia. Among the sites enjoyed by the group were, Independence Park, site of our country's first naval battle, the estate of Henry Cabot Lodge, Prides Crossing, Manchester Yacht Club, Manchester by the Sea, Beverly Farms, Singing Beach, the home of former Governor Christian Herter, and then on to Strombergs Restaurant for a New England Sea Shore Dinner.

After we rested our Cats & filled our tummy's, we were off again for a special viewing of Biff's auto memorabilia collection. To say the least, a good time was had by all.

Sound like fun, well it was. If you live in an interesting place and would like to share something special with your JANE friends, let us know. Trips like this are a great way to get out with our cars, renew friendships & enjoy the beauty that is New England . Hope to see you in October....

And one last comment, on behalf of all JANE members who attended this event.

Thank you Biff Michaud for a great day!

(Due to the change over in Coventry Cat editors, the article wasn't published in the October Coventry Cat. I apologize, ED)



Ed and Kathi Hall outside the museum



The Ciprianos and Mc Gillverys at lunch



JANE members and Dave Reilly's S type

COVENTRY CAT CLASSIFIEDS

For Sale: XK120 parts; 50 items; detailing, chrome, door and tools; Instruments; Wheels and spinners; Manuals and books; Some are NOS, used are in good to excellent condition Larry Berman email lwberman@attbi.com or see the web site <http://www.sover.net/~bcunning/120parts.htm> for a complete list 1002

For Sale: 1967 E type roadster, Red with Black Canvas top; New leather seats and trunk kit; Never rusted; Exterior chrome beautifully redone: Owned it 24 years, 46000 miles; \$30,000.00 Matt Patla 413-562-9298 email mjpatla@webtv.net 1002

Wanted: Wire wheels for XK120; 4 or 5 wheels, 16", 54 spoke with or without tires; tires are 6.00 x 16; Nick at 207-443-5764 1202

For Sale: 1983 Vanden Plus XJ6. 71,000 miles one owner, black w/ cream interior; Beautiful interior, leather is nice wood is great; has car phone and radar detector; good snows all around; body good; some minor rust spots; inspection sticker; runs and drives well; may need front shocks Car at MOT, \$3200; Evan 617-522-6547, Larz Anderson Auto Museum 0103

For Sale: 1969 XKE Jaguar 4.2 Roadster. BRG w/ black interior; new top, detailed; thoroughly restored 5 years ago; superb condition, but a couple of mysteries to entertain you this winter; stated insurance value; appraised for \$33K, sell for best offer; near Boston; call 508-541-6781 or email nspicer@attbi.com for more details 0103

For Sale: 1978 Jaguar XJ12L; NE Regional Concours Champ; Body, paint, interior and mechanicals excellent condition; All new tires; \$4800 or best offer
1993 Chevy Lumina Euro V6, white/red, excellent mechanicals and running gear; power everything; 120K; 1st best offer
1948 MC TC EXU purchased from original owner; Driver for an easy restoration; everything works. Engine burns no oil; needs tires wheel cylinder, paint chrome and clean up or drive as is; make an offer; Call Dave at 603-465-3787 0103

For Sale: 1971 V12 XKE Coupe; Regency red paint; lots of new gaskets and chrome; needs headliner and seat covers; body is very good and paint excellent; stored inside, not on the road at this time; \$15,500; Call 978-568-0171 or e-mail for pictures; lally@wiremachineryco.com 0203

For Sale: 1961 Jaguar MK 2 3.8 Saloon, Only 45,500 miles, 2nd owner; Original black paint w/ tan interior; always garaged, no rust; Perfect for JCNA preservation class 18; All rubber, brakes and systems overhauled at Cloverleaf Auto; drives well with automatic transmission; many NOS spares and car cover included; for delightful photos; can deliver \$20,000 OBO; car is located in SE Penn; may be seen by appointment; call/write Kurt Rappold 610-358-4055 0103

For Sale: Jaguar XK120,140, & 150 new sheetmetal: front and rear wings (patch panels or complete), spats, rear inner wings, fuel tanks, doors, door skins, boot lids, boot lid skins, sills, shut pillars, battery boxes, stainless steel side curtain frames and more! Rebuilding service for your doors, lids and door hinges. All parts fabricated by WES PARTS - over 30 years experience. Call or write Wray Schelin, WES PARTS, 508 347 7749, PO Box 652, Charlton City, MA 01508. For a free

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Letters from the Jaguar Family

From: CHansonjag@aol.com

To: Kurt_Vala@casella.com

Cc: ascurtis@metrocast.net ; harryparkinson@attbi.com

Subject:

Re: Looking for Jag expertise.

Date: Friday, January 10, 2003 7:39AM

Kurt:

Welcome to the area! We have a very large (2nd largest in the US) and active Jaguar club here in New England, the Jaguar Association of New England (JANE). You can find information and actually become a member through our website, <http://www.J-A-N-E.org>. You will find many experts on E-Types and XK120's in the area. I am restoring an XK120FHC. Adrian Curt is our Membership VP, lives not far from Concord (Gilford, NH) and has an XK120DHC that he has worked on extensively, Harry Parkinson, our newsletter editor lives in Nashua and has had a long history with XK's. Frank Williams in Raymond, NH, has several E-Types and knows them. Perhaps the best resource in the club is Gary Hagopian who lives in Sunapee—he knows all Jaguars and is the current president of Jaguar Clubs of North America (JCNA). Rich Mozzetta who has a V12 E type also lives in Sunapee. So you see, you have come to a real Jaguar-active area! I suggest you join JANE, come to our meetings, tech sessions, rallies, driving events, and concours and talk with some of our members. I am sure they can recommend shops with Jaguar expertise in your area. Our meetings are held the fourth Wednesday of every month, starting at 7:00pm, at Skip's Restaurant in Chelmsford, MA, about an hours drive down Rt3 from Concord, a little ways over the MA border from NH.

Carl Hanson

JANE President 2003

From: "Daniel Thompson" <dthompson@gbc.ca>

Subject: JCNA Challenge Championships: are members interested?

Date: Friday, January 24, 2003 10:08 AM

I have taken the liberty of cutting and pasting a few comments that I have seen in a recent email thread discussing the JCNA Challenge Championship:

<SNIP> We may be the only national car club in the United States in which the membership doesn't seem to care about the club's biggest event. Year after year, Triumph and MG and Austin Healey and Corvette and all the others turn out hundreds of cars for Championships. Theirs are ANNUAL. Ours is every other year and we haven't hit 200 cars yet. The Triumph club drew over 250 to Red Wing Minnesota (where?) last summer and their national membership is about one third the size of JCNA. The Austin Healey club of America draws more than 300 every year. Their membership is a little over half of ours.

<SNIP> I looked at the results from the 2001 Franklin Challenge Championship Concours hoping to find a significant number of westerners....well. Guess how many members from the southwest or northwest entered the concours ? ONE

!!!! <SNIP> And...

<SNIP> JCNA members don't care about their national event... the question is why? As you say, other clubs bring in 100s of cars... I think even the Alfa club which held their convention in Franklin a few months before ours in 2001 had more cars there... Alfas !!! how many do you see on the road everyday....

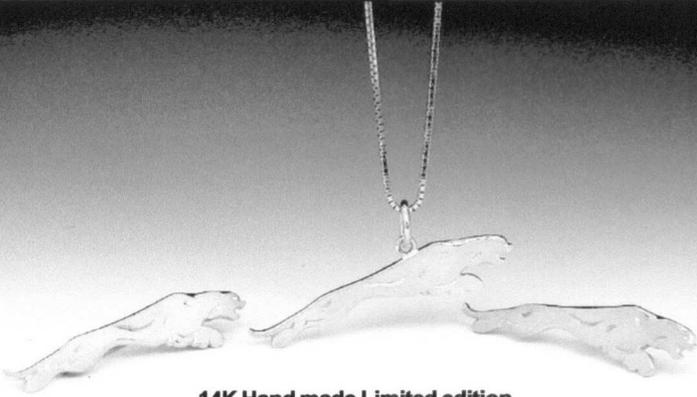
I don't know if this has been looked into before, but it must be. Why are the members not interested in the Challenge Championship? Is it because of locations that are a bit too far? is it the schedule that's geared towards regional participation (attending Phoenix basically blows 2 weeks instead of being able to travel on the weeks ends) ? what is it ? <SNIP>

I'd like to hear the opinions of some JCNA members as to why the National meet, held every two years under the banner of Challenge Championship, fails to attract a significant amount of entrants and participants from a membership of over 5000. Why?

I have my own personal opinions on this but I'd like to hear the opinions of others.

Thanks

Daniel Thompson, NE regional director



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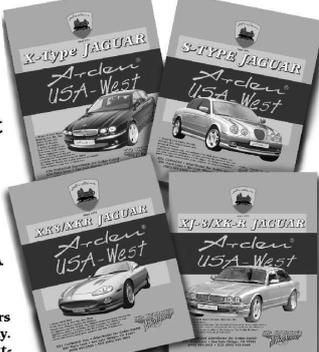
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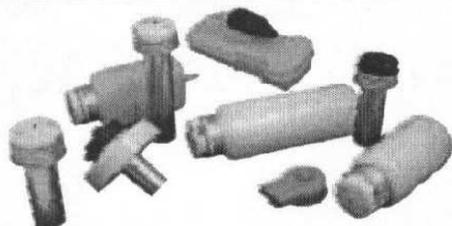
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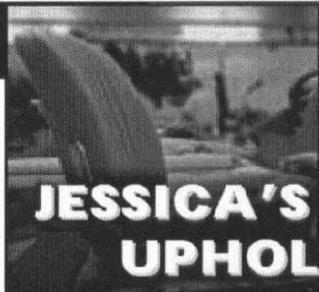
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